

GENERAL NOTES

ALL ELEVATIONS SHOWN ARE REFERRED TO THE N.A.V. DATUM, 1988 ADJUSTED M.S.L.
ANY REFERENCE STANDARDS THROUGHOUT THE PLANS SHALL BE INTERPRETED TO BE THE LATEST STANDARDS OF THE ILLINOIS DEPARTMENT OF TRANSPORTATION AS SHOWN ON THE SCHEDULE OF STANDARDS ON THE COVER SHEET.

WHEREVER IN THE PLANS OR SPECIFICATIONS THE TERM "STANDARD SPECIFICATIONS" IS USED, IT SHALL BE UNDERSTOOD BY THE CONTRACTOR TO MEAN THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" AS PREPARED BY THE DEPARTMENT OF TRANSPORTATION OF THE STATE OF ILLINOIS AND ADOPTED JANUARY 1, 2012

WHEREVER IN THE PLANS OR SPECIFICATIONS THE TERM "STANDARD SPECIFICATIONS FOR WATER MAIN CONSTRUCTION" IS USED IT SHALL BE UNDERSTOOD BY THE CONTRACTOR TO MEAN THE "STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS" AS PREPARED BY I.S.P.E., A.G.C.I., I.M.I. AND U.C.A. ADOPTED JULY 2008.

IT IS THE CONTRACTOR'S RESPONSIBILITY TO ASCERTAIN EXISTING FIELD CONDITIONS BEFORE BIDDING ON THE PROJECT.

THE CONTRACTOR WILL BE REQUIRED TO COMPLY WITH ALL STATE REGULATIONS REGARDING AIR, WATER, AND NOISE POLLUTION.

THE CONTRACTOR SHALL TAKE CARE NOT TO STORE OR DISPOSE OF DEBRIS OR UNSUITABLE MATERIALS WITHIN LIMITS OF THE IMPROVEMENT AND TAKE CARE TO LIMIT CONSTRUCTION TO WITHIN THE RIGHT-OF-WAY AND EASEMENT AREAS.

UTILITY LOCATIONS AND LINES WERE PLOTTED FROM INFORMATION FURNISHED BY THE VARIOUS UTILITY COMPANIES INVOLVED AND THEIR ACCURACY SHOULD BE CONSIDERED APPROXIMATE. NO RESPONSIBILITY IS ACCEPTED FOR THE LOCATIONS OR THAT ALL UTILITY FACILITIES ARE SHOWN. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO DETERMINE THEIR EXACT LOCATION AND TO PROTECT SAME. THE CONTRACTOR WILL ALSO BE RESPONSIBLE FOR AVOIDING CONFLICTS BETWEEN OVERHEAD UTILITY LINES, AND GUY ANCHORS AND THE EQUIPMENT USED FOR EXCAVATING. THESE UTILITY COMPANIES MAY BE ADJUSTING THEIR FACILITIES DURING CONSTRUCTION. THE CONTRACTOR SHALL COOPERATE WITH THE UTILITY COMPANIES WHILE THESE ADJUSTMENTS ARE BEING PERFORMED.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING UTILITY PROPERTY FROM CONSTRUCTION OPERATIONS AS OUTLINE IN ARTICLE 107.31 OF THE STANDARD SPECIFICATIONS. THE JULIE NUMBER IS 800-802-0123. A MINIMUM OF 48 HOUR ADVANCE NOTICE IS REQUIRED. THE FOLLOWING IS A LIST OF UTILITY COMPANIES LOCATED ALONG THIS SECTION. THE UTILITY OWNERS MARKED WITH AN "*" ARE JULIE MEMBERS.

- ILLINOIS AMERICAN WATER (WATER)

• AT&T ILLINOIS (COMMUNICATIONS)

• URBANA AND CHAMPAIGN SANITARY DISTRICT (SANITARY SEWERS)

UC28 (BROADBAND COMMUNICATIONS)
- AMEREN ILLINOIS POWER (ELECTRIC AND GAS)

• COMCAST COMMUNICATIONS (CATV)

• CITY OF URBANA (STORM AND SANITARY SEWERS AND STREETLIGHTS)

EXISTING PAVEMENTS, COMBINATION CURB AND GUTTER OR SIDEWALKS, IN WHICH THE TOP SURFACE IS TO BE BUTT JOINTED TO THE PROPOSED WORK SHALL BE SO JOINED THROUGH FULL DEPTH SAW CUT JUNCTIONS. THIS WORK SHALL BE CONSIDERED AS INCIDENTAL TO THE CONTRACT.

THE EXISTING TIE BARS BETWEEN THE EXISTING PAVEMENT AND EXISTING MEDIANS, GUTTERS AND/OR COMBINATION CURB AND GUTTERS THAT ARE FOUND SUITABLE FOR REUSE SHALL BE CLEANED, STRAIGHTENED, AND INCORPORATED INTO THE NEW CONSTRUCTION. ANY EXISTING TIE BARS THAT ARE FOUND UNSUITABLE TO BE INCORPORATED INTO THE PROPOSED CONSTRUCTION DUE TO EXCESSIVE RUSTING OR DISTRESS SHALL BE REMOVED FLUSH WITH THE FACE OF THE EXISTING CONCRETE AND DISPOSED OF OUTSIDE THE LIMITS OF THE RIGHT-OF-WAY IN ACCORDANCE WITH ARTICLE 202.03 OF THE STANDARD SPECIFICATIONS. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCLUDED IN THE VARIOUS REMOVAL PAY ITEMS AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

HORIZONTAL CONTROL TIES ARE SHOWN FOR THE CONTRACTOR TO PHYSICALLY LOCATE MONUMENTATION IN THE FIELD. IT IS THE CONTRACTOR'S RESPONSIBILITY TO MAINTAIN CONTROL POINTS OR TO USE ADDITIONAL TIES AS NECESSARY TO INSURE THAT CONTROL POINTS CAN BE ACCURATELY REPLICATED DURING CONSTRUCTION.

WHERE SECTION OR SUBSECTION MONUMENTS, BENCHMARKS, OR IRON PIPE MONUMENTS ARE ENCOUNTERED, THE ENGINEER SHALL BE NOTIFIED BEFORE SUCH MONUMENTS ARE REMOVED. THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL MONUMENTS UNTIL AN ILLINOIS REGISTERED LAND SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATION. THE CONTRACTOR WILL BE RESPONSIBLE FOR HAVING AN ILLINOIS REGISTERED LAND SURVEYOR RE-ESTABLISH ANY MONUMENTS DESTROYED OR DISTURBED BY HIS OPERATIONS.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR CLEANUP OF THE SITE PRIOR TO FINAL ACCEPTANCE IN ACCORDANCE WITH ARTICLE 104.06 OF THE STANDARD SPECIFICATIONS. THIS WORK SHALL ALSO INCLUDE CLEANING ALL DRAINAGE FACILITIES OF FOREIGN MATERIALS.

THE EXCAVATION FOR THIS PROJECT IS CLASSIFIED AS EARTH EXCAVATION IN ACCORDANCE WITH SECTION 202 OF THE STANDARD SPECIFICATIONS. THE EARTH EXCAVATION SHALL INCLUDE THE REMOVAL OF EARTH AND UNCLASSIFIED MATERIALS AND THE TRANSPORTATION AND PLACEMENT OF SUITABLE EXCAVATED MATERIALS BEHIND THE BACK OF CURB.

THE EXISTING SIGNS, MAIL BOXES AND MISCELLANEOUS ITEMS WHICH INTERFERE WITH THE CONSTRUCTION SHALL BE CAREFULLY REMOVED AND RESET OR RETURNED TO THE CITY OF URBANA'S PUBLIC WORKS GARAGE AT 706 S. GLOVER AVE. FOR REINSTALLATION BY CITY CREWS. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCLUDED IN THE VARIOUS REMOVAL PAY ITEMS AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

IF THE PROPOSED STORM SEWER WORK CREATES CONFLICTS WITH EXISTING UTILITIES, THE ENGINEER WILL DETERMINE THE COURSE OF ACTION. THE CONTRACTOR SHALL COOPERATE WITH THE UTILITY COMPANIES WHEN ADJUSTMENTS MUST BE MADE AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR ANY DELAYS, INCONVENIENCE OR DAMAGE SUSTAINED BY THE CONTRACTOR AS A RESULT.

ALL CASTINGS REMOVED DURING CONSTRUCTION ARE THE PROPERTY OF THE CITY OF URBANA AND SHALL BE NEATLY STORED AT A LOCATION DESIGNATED BY THE ENGINEER UNTIL THEY CAN BE HAULED TO THE CITY'S PUBLIC WORKS DEPARTMENT BY CITY FORCES.

THE CONTRACTOR SHALL CAREFULLY PROTECT ANY TREES OR SHRUBS NOT INCLUDED IN THE CONTRACT FOR REMOVAL. SNOW FENCE SHALL BE ERECTED AT THE DRIP LINE OF THE TREES AND SHRUBS TO REMAIN THAT ARE IMMEDIATELY ADJACENT TO THE WORK FOR PROTECTION DURING CONSTRUCTION OPERATIONS. THE COST OF THIS WORK SHALL BE INCLUDED IN THE PAY ITEM FOR EARTH EXCAVATION WITH NO ADDITIONAL COMPENSATION ALLOWED.

THIS PROJECT IS GOVERNED BY THE CITY'S N.P.D.E.S. GENERAL PERMIT NO. ILR 400482. THIS PERMIT WILL REQUIRE EROSION AND SEDIMENT CONTROL. THE CONTRACTOR WILL BE REQUIRED TO PROVIDE INLET AND PIPE PROTECTION PER HIGHWAY STANDARD 280001-02 EXCEPT STRAW BALES SHALL NOT BE ALLOWED. THE LOCATIONS FOR THE INLET AND PIPE PROTECTION SHALL BE AT ALL DRAINAGE STRUCTURES WITHIN THE LIMITS OF CONSTRUCTION AND ANY DOWNSTREAM STRUCTURES THAT HAVE THE POTENTIAL TO RECEIVE ANY WATER BORNE SILT OR RUNOFF. A QUANTITY FOR INLET AND PIPE PROTECTION HAS BEEN INCLUDED IN THE PROJECT AND SHALL INCLUDE THE USE OF EITHER INLET BASKETS OR FILTERS AS NEEDED.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING EXISTING PAVEMENT SURFACES TO INCLUDE CONSTRUCTION OF AGGREGATE ENTRANCES FOR CONTROLLING TRACKING, AND CLEANING MUD AND DEBRIS FROM SURFACES WITH NO ADDITIONAL COMPENSATION.

THE FINISHED EARTHWORK SHALL HAVE A VEGETATION SUSTAINING SOIL COVERING THE TOP SIX INCHES IN AREAS TO BE SEEDDED. THE CONTRACTOR SHALL STOCKPILE SUITABLE SOILS FROM THE EXCAVATION OPERATIONS OR SUPPLY SUITABLE TOPSOIL AS REQUIRED. THE SOIL MATERIALS SHALL MEET THE REQUIREMENTS OF ARTICLE 1061.06(a) OF THE STANDARD SPECIFICATIONS OR BE APPROVED BY THE ENGINEER. THE TOPSOIL REQUIRED TO BE FURNISHED WILL BE PAID FOR SEPARATELY. ANY FINAL SHAPING REQUIRED SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCIDENTAL TO SEEDING.

FERTILIZING, SEEDING AND MULCH SHALL BE DONE AT LOCATIONS SHOWN ON THE PLANS WHERE THE EXISTING EARTH HAS BEEN DISTURBED AND AT LOCATIONS WITHIN THE RIGHT-OF-WAY HAVING INSUFFICIENT VEGETATION AS DIRECTED BY THE ENGINEER. ANY EXISTING AREAS OUTSIDE THE LIMITS OF CONSTRUCTION DAMAGED BY THE CONTRACTOR SHALL BE SEEDDED AT THEIR OWN EXPENSE AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED. SEEDING SHALL BE CLASS 1 WITH MULCH METHOD 3. NO STRAW WILL BE ALLOWED AS MULCH. RATES OF APPLICATIONS FOR PREPARATION OF SEEDING:

- NITROGEN FERTILIZER NUTRIENTS = 80 LB. PER ACRE

PHOSPHORUS FERTILIZER NUTRIENTS = 80 LB. PER ACRE

POTASSIUM FERTILIZER NUTRIENTS = 80 LB. ACRE

MULCH = 2 TON PER ACRE

THE PROPOSED PAVEMENT CONSISTS OF A NON-REINFORCED 7.5 INCH THICK PORTLAND CEMENT CONCRETE PAVEMENT(JOINTED) WITH TYPE B-6.12 COMBINATION CONCRETE CURB AND GUTTER AS SHOWN IN THE PLANS AND PROPOSED TYPICAL SECTIONS. THE PAVEMENT AND CURBS SHALL CONFORM TO THE REQUIREMENTS OF THE LATEST ILLINOIS DEPARTMENT OF TRANSPORTATION HIGHWAY STANDARDS 420001 AND 606001. ALL SAWED JOINTS SHALL BE 73 IN DEPTH AND SEALED WITH HOT POLYED MATERIAL IN ACCORDANCE WITH SECTION 420.12 OF THE STANDARD SPECIFICATIONS. ALL SAWED JOINTS SHALL BE SAWED AS SOON AS THE CONCRETE HAS HARDENED SUFFICIENTLY TO PERMIT SAWING WITHOUT EXCESSIVE RAVELING. THE FINAL FINISH OF THE PAVEMENT SURFACE SHALL BE TYPE B.

HOT MIX ASPHALT MIX SPECIFICATIONS

MIXTURE USE	INCIDENTAL
AC/PG	PG 64-22
RAP % (MAX)	15
DESIGN AIR VOIDS	4.0% @ N _{max} =50
MIX COMP(GRADATION)	IL 9.5
FRICITION AGGREGATE	MIX "C"

HORIZONTAL CONTROL POINTS

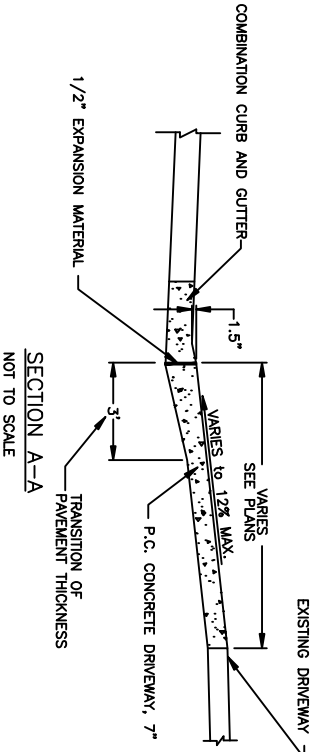
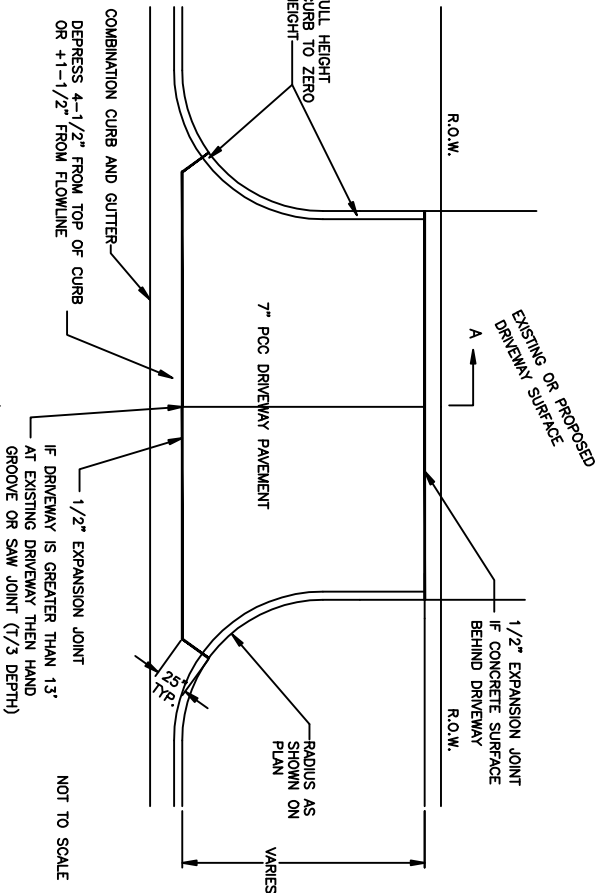
ST.	CONTROL ATTRIBUTE	PHYSICAL FEATURE	STATION	NORTHING	EASTING
50	POINT ON BASELINE	C.L. OF EX. BIT. P/WMT.	STA. 0+00.00, 0.00' RT	1263145.46	1021315.17
	POINT ON BASELINE	C.L. OF EX. OIL & CHIP	STA. 3+00.00, 0.00' RT	1263445.43	1021311.29
	POINT ON BASELINE	C.L. OF EX. OIL & CHIP	STA. 6+30.00, 0.00' RT	1263775.40	1021307.01

SUMMARY OF QUANTITIES

CODED PAY

ITEM NO.	ITEM	UNIT	QUANTITY
20100110	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	IN DIA	113
20100210	TREE REMOVAL (OVER 15 UNITS DIAMETER)	IN DIA	64
20200100	EARTH EXCAVATION	CU YD	520
21000310	GRANULAR EMBANKMENT, SPECIAL	CU YD	170
21001000	GEOTECHNICAL FABRIC FOR GROUND STABILIZATION	SQ YD	1530
21101600	TOPSOIL FURNISH AND PLACE, VARIABLE DEPTH	SQ YD	1612
25000100	SEEDING, CLASS 1	ACRE	0.33
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	30
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	30
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	30
25100125	MULCH, METHOD 3	ACRE	0.33
28000500	INLET AND PIPE PROTECTION	EACH	32
35102000	AGGREGATE BASE COURSE, TYPE B, 8"	SQ YD	64
35300300	PORTLAND CEMENT CONCRETE BASE COURSE, 8"	SQ YD	16.5
40800010	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	34
40800050	INCIDENTAL HOT-MIX ASPHALT SURFACING	TON	12
42000201	PORTLAND CEMENT CONCRETE PAVEMENT, 7 1/2" (JOINTED)	SQ YD	1327
42300300	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 7" PAVEMENT REMOVAL	SQ YD	262
44000100	DRIVEWAY PAVEMENT REMOVAL	SQ YD	312
44000200	COMBINATION CURB AND GUTTER REMOVAL	FOOT	280
44000500	AGGREGATE SHOULDERS, TYPE B, 8"	FOOT	62
48101600	PIPE CULVERT REMOVAL	SQ YD	13.4
50105220	STORM SEWERS, CLASS A, TYPE 1, 12"	FOOT	250
550A0050	STORM SEWERS, CLASS B, TYPE 1, 8", PVC TRUSS PIPE	FOOT	122
550B0030	STORM SEWERS, CLASS B, TYPE 1, 15", PVC TRUSS PIPE	FOOT	15
550B0070	CONTROLLED LOW-STRENGTH MATERIAL	EACH	658
58300100	MANHOLES, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, OPEN LID	EACH	90
60218300	MANHOLES, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, OPEN LID	EACH	4
60219000	MANHOLES, TYPE A, 4'-DIAMETER, TYPE 8 GRATE	EACH	1
60236200	INLETS, TYPE A, TYPE 8 GRATE	EACH	5
60236800	INLETS, TYPE A, TYPE 11 FRAME AND GRATE	EACH	4
60255500	MANHOLES TO BE ADJUSTED	EACH	2
60500040	REMOVING MANHOLES	EACH	2
60603900	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6,12	EACH	1018
X0325582	REMOVE AND REPLACE STONE RIPRAP	CU YD	8
X5427800	REMOVE AND RELOCATE END SECTIONS	EACH	2
X6022210	MANHOLES, TYPE A, 4'-DIAMETER WITH SALVAGED FRAME AND GRATE	EACH	2
X7010216	TRAFFIC CONTROL AND PROTECTION (SPECIAL)	L SUM	1
X0003315	SUBGRADE REMOVAL AND REPLACEMENT	CU YD	40
Z0013786	CONSTRUCTION LAYOUT	L SUM	1
#5000901	STORM SEWERS, CLASS B, TYPE 1, 15" PVC TRUSS PIPE X 8" PVC TRUSS PIPE BRANCH	EACH	1
#5000902	INLETS, TYPE A, TYPE 11 FRAME AND GRATE (SPECIAL)	EACH	2

STRUCTURAL PAVEMENT DESIGN INFORMATION
<div>WILLOW ROAD PAVEMENT CONSTRUCTION</div> <div>PROPOSED PAVEMENT DESIGN LIFE = 30 YEARS</div> <div>STRUCTURAL DESIGN TRAFFIC: YEAR 2027</div> <div>PV = 600 SU = 70 MU = 60</div> <div>ROAD/STREET CLASSIFICATION: CLASS III (MOD)</div> <div>PERCENT OF STRUCTURAL DESIGN TRAFFIC IN DESIGN LANE: P = 50%, S = 50%, M = 50%</div> <div>TRAFFIC FACTOR: TF = 0.86</div> <div>SUBGRADE SUPPORT RATING: BR = "Z"ASSUMED</div> <div>MINIMUM STRUCTURAL DESIGN REQUIREMENTS: P.C. CONCRETE PAVEMENT = 7 1/2" NONREINFORCED SUBBASE = GEOTECHNICAL FABRIC FOR IMPROVED SUBGRADE</div>



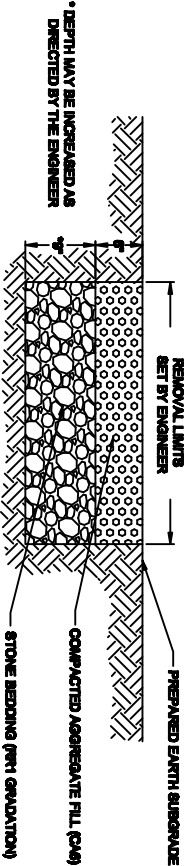
P.C. CONCRETE DRIVEWAY GENERAL NOTES

THE COST OF CONSTRUCTING THE P.C. CONCRETE DRIVEWAY PAVEMENT THICKER ADJACENT TO THE COMBINATION CONCRETE CURB AND GUTTER AS SHOWN IN SECTION A-A WILL BE CONSIDERED INCIDENTAL TO THE P.C. CONCRETE DRIVEWAY PAVEMENT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

WHEN THE WIDTH OF THE P.C. CONCRETE DRIVEWAY IS 18' OR GREATER A CONTRACTION JOINT, TYPE APPROVED BY THE ENGINEER, SHALL BE PLACED IN THE CENTER OF THE DRIVEWAY OR AS SHOWN ON THE PLAN.

ALL COSTS OF CONSTRUCTING THE DEPRECEATED PORTION OF THE CURB AND GUTTER AS SHOWN SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE PER FOOT FOR CURB AND GUTTER OF THE TYPE SPECIFIED IN THE PLAN.

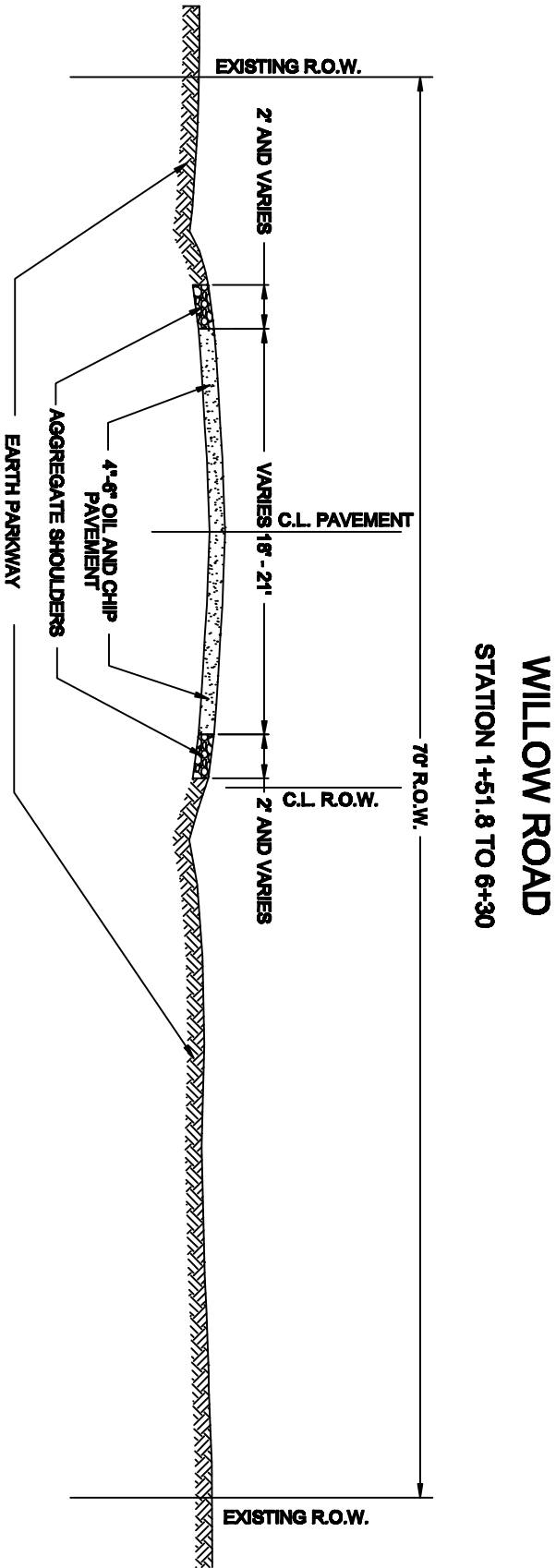
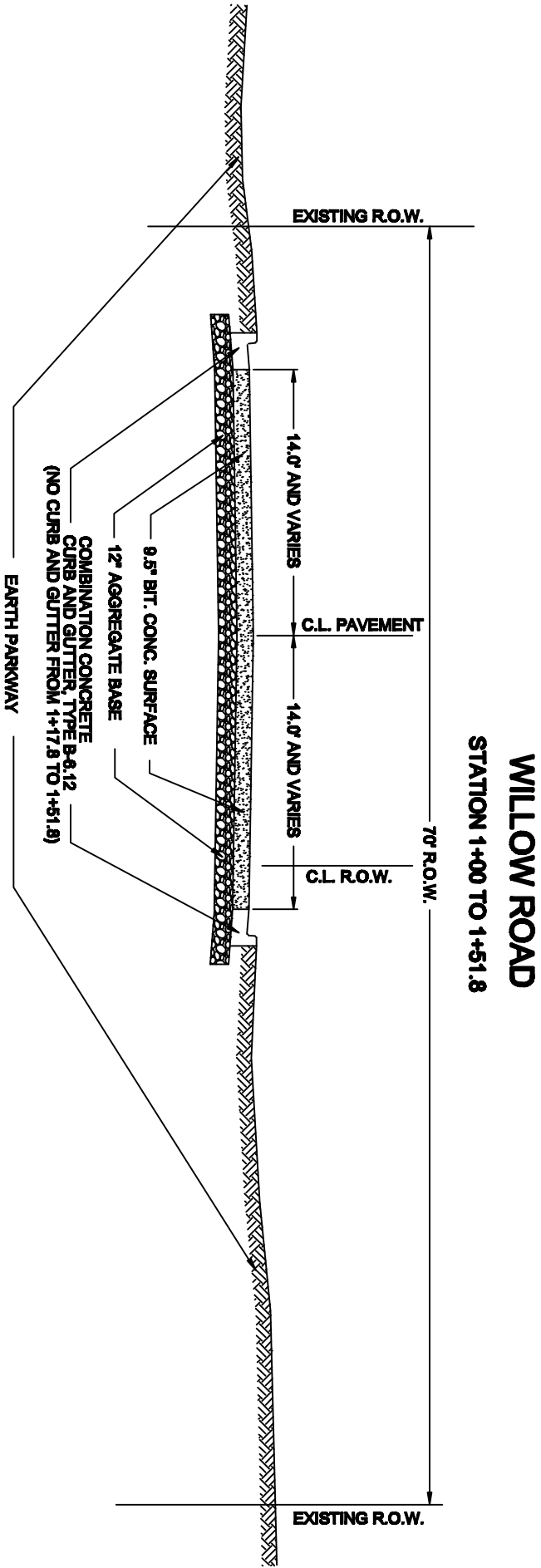
ALL P.C. CONCRETE DRIVEWAY PAVEMENTS SHALL BE 7" THICK.



SUBGRADE THAT HAS BEEN PREPARED AND COMPLETED WITH NORMAL REPORT AND STILL DOES NOT MEET THE REQUIRED DENSITY AND STABILITY SHALL BE REEVALUATED BY REMOVING AND DISPOSING OF THE SOFT AND UNSTABLE MATERIAL, AND REPLACED WITH SELECT AGGREGATE MATERIAL. LIMITS AND LOCATIONS ARE TO BE DETERMINED BY THE ENGINEER.

ALL COSTS OF SUBGRADE REEVALUATION INCLUDING EARTH EXCAVATION, PLACEMENT AND COMPACTION OF R11 AND C4-6 AGGREGATE BASE MATERIALS SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE PER CURB YARD FOR SUBGRADE REMOVAL AND REPLACEMENT AT LOCATIONS DESIGNATED BY THE ENGINEER.

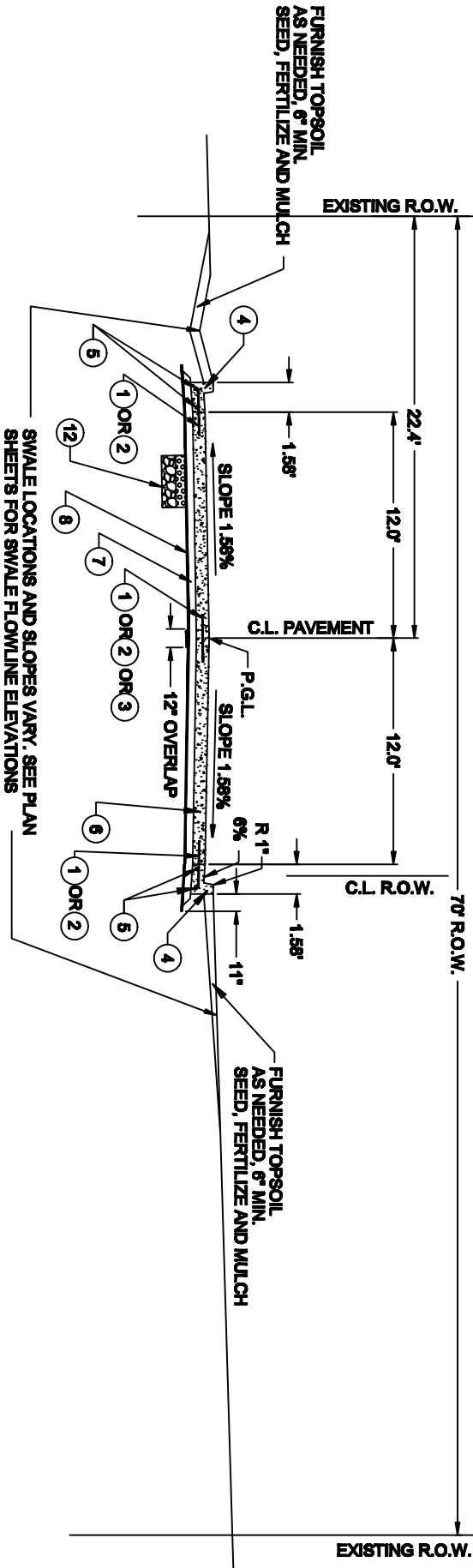
EXISTING TYPICAL CROSS-SECTIONS



PROPOSED TYPICAL SECTIONS

WILLOW ROAD

STATION 1+00.0 TO 1+51.8
(Width varies from 30.0' e-e to 24.0' e-e)
STATION 1+51.8 TO 5+95

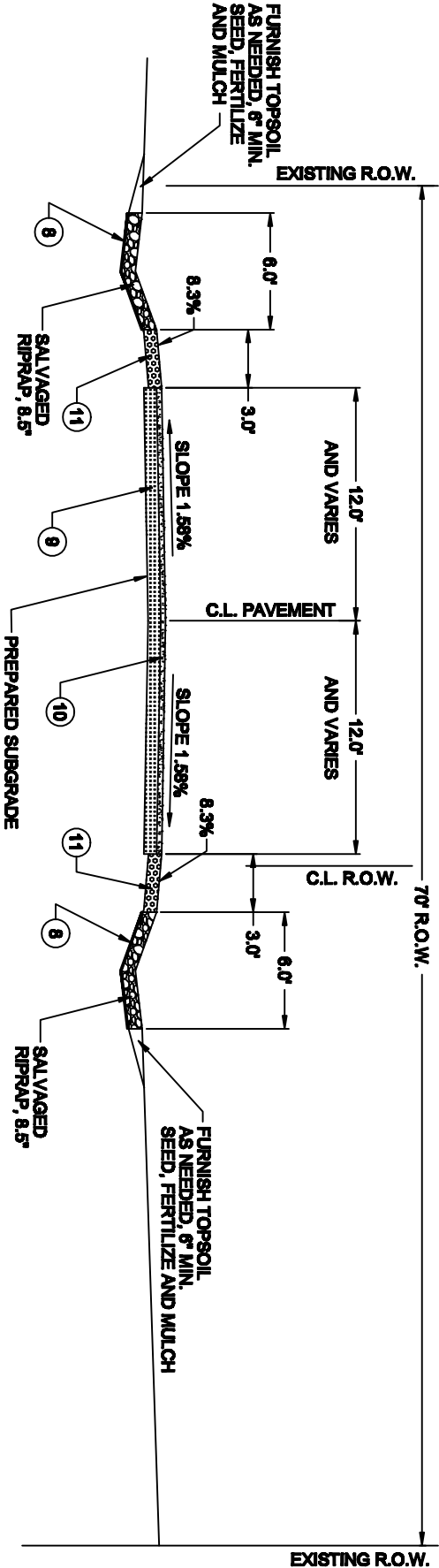


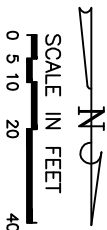
PROPOSED TYPICAL SECTION KEY

- 1 LONGITUDINAL CONSTRUCTION JOINT WITH #6 TIE BARS 30' LONG @ 24' CENTERS FORMED IN PLACE (STD. 420001-07)
- 2 LONGITUDINAL CONSTRUCTION JOINT WITH #6 TIE BARS 24' LONG @ 24' CENTERS GROUTED IN PLACE (STD. 420001-07)
- 3 LONGITUDINAL SAWED JOINT WITH #6 TIE BARS ON SUPPORTS #6 TIE BARS 30' LONG @ 24' CENTERS (STD. 420001-07)
- 4 COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12 (STD. 606001-04)
- 5 SAWED CONTRACTION JOINTS 2 1/2' DEEP WITH (2) 1-1/4' DIAMETER X 18' LONG SMOOTH DOWEL BARS 12' APART ON 15' CENTERS.
- 6 P.C. CONCRETE PAVEMENT, 7.5' (JOINTED) (STD. 420101-04) MODIFIED TO VARIABLE LANE WIDTH (12'-15') FROM STA. 1+00 TO 1+60
- 7 GRANULAR EMBANKMENT, SPECIAL (R.A.P. MATERIAL)
- 8 GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
- 9 AGGREGATE BASE COURSE, TYPE B, 8"
- 10 INCIDENTAL HOT-MIX ASPHALT SURFACE, 3", IL-9.5, MIX C, N50 BITUMINOUS MATERIALS (PRIME COAT), MC-30
- 11 AGGREGATE SHOULDERS, TYPE B, 8"
- 12 SUBGRADE REMOVAL AND REPLACEMENT AS DIRECTED BY THE ENGINEER STONE BEDDING, 9" (RRT GRADATION), AGGREGATE CAP, 6" (CA-6 GRADATION)

WILLOW ROAD

STATION 5+95 TO 6+15





EARTHWORK NOTES:

1. THE INDICATED EARTHWORK VOLUMES ARE ESTIMATES BASED ON THE "AVERAGE END AREA" METHOD OF CALCULATION. CROSS SECTION/ASPHALT DRAWINGS ARE AVAILABLE UPON REQUEST. EARTHWORK VOLUMES WILL VARY WITH ACTUAL SOIL CONDITIONS ENCOUNTERED DURING CONSTRUCTION. THESE ESTIMATES ARE PROVIDED FOR THE CONVENIENCE OF THE CONTRACTOR AND SHOULD BE CONSIDERED APPROXIMATE.
2. EXCESS SURFACE EARTH MATERIALS FROM SEWER TRENCHES AND EXCAVATIONS FOR STRUCTURES MAY BE USED FOR FILL TO WITHIN 8 INCHES OF FINL GRADE. CLEAN TOPSOIL EXCAVATED FROM WITHIN THE CONSTRUCTION LIMITS MAY BE USED AS FINISH TOPSOIL, WITH APPROVAL FROM THE ENGINEER.
- ADDITIONAL TOPSOIL FROM SOURCES OFFSITE WILL BE NEEDED. UNCLASIFIED OR OIL AND CHIP OR AGGREGATE MATERIAL SHALL BE CONSIDERED AS EARTH EXCAVATION AND DISPOSED OF OFFSITE BY THE CONTRACTOR. THE COST OF DISPOSAL OF EXCESS EARTH MATERIALS WILL NOT BE PAID FOR SEPARATELY AND SHALL BE INCLUDED IN THE COST OF EARTH EXCAVATION.

EARTHWORK CALCULATIONS					
STA.	Excavation (CU)	Backfill Placement (FILL)	Topsoil Placement (FILL)	Earth Excavation Volume	Topsoil Placement Volume
1+00	0	0	0	0	0
1+10	0	0	0	0	0
1+20	0	0	0	0	0
1+30	0	0	0	0	0
1+40	0	0	0	0	0
1+50	0	0	0	0	0
1+60	0	0	0	0	0
1+70	0	0	0	0	0
1+80	0	0	0	0	0
1+90	0	0	0	0	0
2+00	0	0	0	0	0
2+10	0	0	0	0	0
2+20	0	0	0	0	0
2+30	0	0	0	0	0
2+40	0	0	0	0	0
2+50	0	0	0	0	0
2+60	0	0	0	0	0
2+70	0	0	0	0	0
2+80	0	0	0	0	0
2+90	0	0	0	0	0
3+00	0	0	0	0	0
3+10	0	0	0	0	0
3+20	0	0	0	0	0
3+30	0	0	0	0	0
3+40	0	0	0	0	0
3+50	0	0	0	0	0
3+60	0	0	0	0	0
3+70	0	0	0	0	0
3+80	0	0	0	0	0
3+90	0	0	0	0	0
4+00	0	0	0	0	0
4+10	0	0	0	0	0
4+20	0	0	0	0	0
4+30	0	0	0	0	0
4+40	0	0	0	0	0
4+50	0	0	0	0	0
4+60	0	0	0	0	0
4+70	0	0	0	0	0
4+80	0	0	0	0	0
4+90	0	0	0	0	0
5+00	0	0	0	0	0
5+10	0	0	0	0	0
5+20	0	0	0	0	0
5+30	0	0	0	0	0
5+40	0	0	0	0	0
5+50	0	0	0	0	0
5+60	0	0	0	0	0
5+70	0	0	0	0	0
5+80	0	0	0	0	0
5+90	0	0	0	0	0
6+00	0	0	0	0	0
6+10	0	0	0	0	0
6+20	0	0	0	0	0
6+30	0	0	0	0	0
6+40	0	0	0	0	0
6+50	0	0	0	0	0
6+60	0	0	0	0	0
6+70	0	0	0	0	0
6+80	0	0	0	0	0
6+90	0	0	0	0	0
7+00	0	0	0	0	0
7+10	0	0	0	0	0
7+20	0	0	0	0	0
7+30	0	0	0	0	0
7+40	0	0	0	0	0
7+50	0	0	0	0	0
7+60	0	0	0	0	0
7+70	0	0	0	0	0
7+80	0	0	0	0	0
7+90	0	0	0	0	0
8+00	0	0	0	0	0
8+10	0	0	0	0	0
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8+40	0	0	0	0	0
8+50	0	0	0	0	0
8+60	0	0	0	0	0
8+70	0	0	0	0	0
8+80	0	0	0	0	0
8+90	0	0	0	0	0
9+00	0	0	0	0	0
9+10	0	0	0	0	0
9+20	0	0	0	0	0
9+30	0	0	0	0	0
9+40	0	0	0	0	0
9+50	0	0	0	0	0
9+60	0	0	0	0	0
9+70	0	0	0	0	0
9+80	0	0	0	0	0
9+90	0	0	0	0	0
10+00	0	0	0	0	0

CREATIVE THERMAL SOLUTIONS

2209 North Willow Road

TREE REMOVAL, 6"-15"			
STA.	IN. DIA.	IN. DIA.	IN. DIA.
2+65, 20' RT	11		
2+90, 21' RT	9		
3+05, 20' RT	8		
3+12, 22' RT	6		
3+20, 18' RT	6		
3+25, 19' RT	12		
3+30, 17' RT	8		
3+35, 18' RT	6		
3+40, 21' RT	13		
3+45, 19' RT	7		
3+52, 20' RT	13		
3+58, 18' RT	8		
3+65, 18' RT	6		
TOTAL THIS SHEET	113		

PAVEMENT REMOVAL			
STA.	IO	STA.	IO
0+71.8	1+51.7		
3+87.3	4+91.2		
TOTAL THIS SHEET	13.13		

DRIVEWAY PAVEMENT REMOVAL			
STA.	IO	STA.	IO
2+06, LT	51.4		
2+09, RT	65.6		
2+52, LT	6.5		
3+94, RT	77.0		
5+62, RT	79.5		
TOTAL THIS SHEET	280.0		

REMOVING MANHOLES			
STA.	IO	STA.	IO
0+42.2, 20x7' LT	1		
0+77.2, 24x4' LT	1		
TOTAL THIS SHEET	2		

